

AVALON POINT PLANNED DEVELOPMENT

Goose Creek, South Carolina

March 28, 2023



DEVELOPED BY:
Beazer Homes
4401 Belle Oaks Drive
Suite 120
North Charleston, SC 29405

PREPARED BY:
Stantec
4969 Centre Pointe Drive
Suite 200
North Charleston, SC 29418

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1. STATEMENT OF OBJECTIVES & COMMUNITY BENEFITS

The goal of the Avalon Point Planned Development (APPD) is to expand within the existing residential area with townhome and single-family neighborhoods with commercial opportunities abutting Red Bank Road. The total acreage of the development is 34.35 acres per the property card. The site is bound to the west by Red Bank Road, to the east and south by single family homes, and to the north by both commercial and single-family homes. There will be two access points to the site. A right in/right out entry at Red Bank Road and a full entry at Wisteria Road. The neighborhood will complement the existing adjacent commercial and residential uses and will help propel further enhancements of the surrounding area.

COMMUNITY BENEFITS:

The Avalon Point Planned Development is located within the City of Goose Creek's Red Bank District (RBD) and will provide several community benefits to the existing residents and commercial businesses as well visitors. Overall, the proposed master plan will expand upon the communities existing single family housing market, integrate alternative modes of transportation and provide opportunities to invite retail and commercial businesses to the District.

The master plan has taken into consideration existing vehicular/pedestrian connections, hydrology (internal and external), maximized open space possibilities with existing utility and drainage easements as amenities and treatment of Red Bank Road frontage to enhance the corridor.

Provided benefits for Avalon Point PD include but are not limited to:

- A network of trails and specifically a trail connection from Red Bank Road to Pandora Drive to connect residents/visitors to the existing bike lane at Pandora and Wisteria Drive. This facility provides connections to adjacent public roads.
- Multiple parks within the community that can be accessed by different modes of transportation.
- Sidewalks that provide access along the private right of ways or in other common areas of the master plan. These connections will connect users to buildings, parking areas, open space and public streets.
- Park furnishings and amenities such as pavilions, fire pits, playground, dog park, sensory gardens, bike parking (on and off street)
- Safe pedestrian connections with crosswalks, signage, and other safety features
- Connected stormwater system
- Opportunities to pull buildings closer to Red Bank Road.
- Wayfinding/signage
- Integration of art into building facades and signage.

2. DEVELOPMENT STANDARDS

Property Address:	117 Red Bank Road Goose Creek, SC 29445
TMS/Site Area:	City of Goose Creek & Berkeley County, SC TMS#244-09-02-008 (32.26 acres)
PD Area:	+/- 34.35 Acres
Existing Zoning:	GC General Commercial (City of Goose Creek)
Proposed Zoning:	Planned Development (Avalon Point)
Existing Use:	Undeveloped
Proposed Use:	Single Family Residential Attached, Detached & Commercial
Proposed Maximum Units:	142 Units Single Family – Detached – 71 Townhome – Attached – 71
Proposed Maximum Building Coverage:	Residential - 75% of Lot Commercial - 100%
Minimum Open Space:	A minimum of 20% of the site. $+/- 34.35 \text{ AC} \times .20 = 6.8 \text{ AC}$ minimum.

Any items not addressed in the planned development document shall revert to the most current version of the City Ordinance at time of final approval.

3. PROPOSED LAND USE

LAND USE TABLE

Total Planned Development Acreage	+/- 34.35 Acres per property card/+/- 32.61 per survey
Commercial Acreage	+/- 3.5 Acres (includes ponds and drainage easements)
Total Single Family Detached and Attached Acreage	+/- 29.11 Acres (includes ponds, wetlands, and drainage easements)
Proposed Residential Lots	142 Residential Lots
Proposed Single Family	71 Lots
Proposed Townhomes	71 Units
Proposed Open Space Areas	Combination of active/passive spaces including.
Wetland Area	1.2 Acres
50' Private right of way	+/- 4.5 Acres
20' Private Alley right of way	+/-1.5 Acres
Park Amenity Spaces	+/-2.5Acres (open space)
Santee Cooper Easement	3.2 Acres (open space)
Pond Areas	+/- 2.5 Acres (open space)
Buffers	+/- 1.8 Acres (open space)
<i>Disclaimer: Land use acreages are approximate and will be finalized with platting and approval by City Zoning Administrator</i>	

LOT DIMENSIONAL STANDARDS

Single Family Detached	4,200 Square Feet (Minimum Lot Area)
Minimum Lot Width	42 Feet
Minimum Lot Depth	100 Feet
<i>Building Setbacks</i>	
Front	20 Feet (Stairs & Roof overhangs allowed in setback)
Side	5 Feet. Roof Overhangs, HVAC units, meters and electric panels allowed in setbacks.
Rear	10 Feet. Roof Overhangs, HVAC units, meters and electric panels allowed in setbacks

3. PROPOSED LAND USE

<i>Maximum Building Coverage</i>	75%
<i>Maximum Building Height</i>	35 Feet to peak of roof (from average foundation grade)
Single Family Attached(Townhomes)	1,500 Square Feet (Minimum Lot Area)
Minimum Lot Width	20 Feet
Minimum Lot Depth	75 Feet
<i>Building Setbacks</i>	
Front	10 Feet (stairs & roof overhang allowed in setback)
Side (end units)	5 Feet. 10' minimum between building units. Roof Overhangs, HVAC units, meters and electric panels allowed in setbacks.
Rear	10 Feet (Roof Overhangs, HVAC units, meters and electric panels allowed in setbacks)
<i>Maximum Building Coverage</i>	75%
<i>Maximum Building Height</i>	45 Feet to peak of roof (from average foundation grade)
	<i>Disclaimer: LOT DIMENSION STANDARDS may be modified up to 15% upon Approval by the City Zoning Administrator</i>
COMMERCIAL LOT AREA	
<i>Minimum area</i>	N/A
Minimum lot Width	N/A
Minimum lot Depth	N/A
<i>Building Setbacks</i>	N/A
<i>Buffers</i>	See Section 8 on design guidelines for buffers
<i>Maximum Building Coverage</i>	100%
<i>Maximum Building Height</i>	2 stories - 35 Feet to peak of roof (from average foundation grade)
<i>Land Use</i>	See Section 8 for Commercial Land Use

Disclaimer: COMMERCIAL LOT AREA STANDARDS may be modified up to 15% upon Approval by the City Zoning Administrator

4. INFRASTRUCTURE AVAILABILITY

The project will connect to existing water, sewer, storm drainage and electrical infrastructure that are adjacent to the property. The surrounding properties are a mixture of single family, civic and commercial uses.

Water/Sewer: Water and sewer for the development will connect to the existing public water and sewer mains adjacent to the site. Water and sewer will be installed in the new proposed right of way to service the proposed development. Water will be provided by the City of Goose Creek Water. Sewer service will be provided by Berkeley County Water & Sanitation.

Roads: The primary access to the site will be via Wisteria Road and Red Bank Road. The applicant will prepare a Traffic Impact Analysis (TIA) and submit it to SCDOT to determine if any improvements need to be made to Red Bank Road or Wisteria Road. Roads internal to the development will have a proposed 50' private right of way (ROW) with curb and gutter and sidewalks on both sides. On-street parking to be provided generally in accordance with project master plan.

Stormwater: The overall site area is +/- 34.35 ac per property card (+/-32.61 per survey) of undeveloped land in the City of Goose Creek, SC. Today, the site is primarily wooded with a 100' Santee Cooper Easement and an existing open drainage system that runs through the property. There are no stormwater controls on the site today. The proposed development will collect stormwater via a system of pipes and ditches and route it to one of five proposed stormwater retention ponds. These ponds will be sized to manage the potential for increased runoff from the development. Stormwater design will be in accordance with City, County and State regulations. The new stormwater ponds and drainage easements will be owned by Berkeley County and maintained by the new Home Owners Association.

Power: Power will be provided by Berkeley Electric Cooperative.

5. Schedule

Upon approval of the planned development by the City of Goose Creek, applications will be made for final site plan and subdivision plans for the project. At this time, it is anticipated it will take 2 to 3 years to complete site improvements and approximately 5 to 7 years to complete project build out.

6. ACCESS

All proposed residential lots will have direct driveway access to the proposed 50' private rights-of-ways and the 20' private alleyway garage access within the Townhome portion of the development. The overall planned development will have two points of access, which will be to Red Bank Road and Wisteria Road. The commercial parcel(s) will not have direct access onto Red Bank Road but will access via the internal roadway.

7. OPEN SPACE

This development has been designed to maximize the open space experience and is laid out to ensure these spaces are easily accessible and beneficial for the community. The Zoning Administrator shall review final development plans for implementation of the elements described in this section. The photos and graphics shown are precedent images to convey the intent of these elements. The open space amenities are as defined below:

The 100' Santee Cooper Easement - This easement bisects the planned development and shall be integrated into the open space plan to create a large greenspace with a 10' multi-use trail. This trail will provide pedestrians and bicyclist connections from Red Bank Road to Pandora Drive, which will connect to the greater bike lane route within the community.

No permanent vertical structures shall be constructed in the easement, however low pollinator gardens, mowed lawn areas and natural meadows shall be included. The proposed multi-use trails and neighborhood paths shall be either asphalt, concrete and/or pervious paving with pedestrian crossings where trails intersect with roadways.

Santee Cooper has reviewed the master plan and is in conceptual agreement. Final approval from Santee Cooper will be provided upon submission of final site plan and construction drawings.



Example of asphalt path/trail adjacent to pond



Example of concrete path

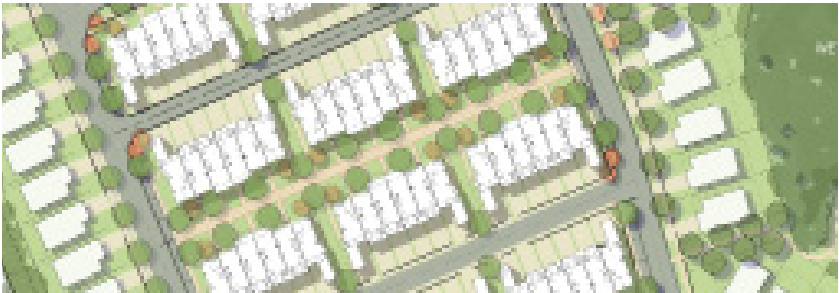


7. OPEN SPACE

Central Community Amenity - This open space amenity includes trails, open lawns and gathering spaces for the planned development. The community amenities include: an open-air pavilion, fire pit, children's play area, and a system of trails and paths that connect internally throughout the neighborhood, to Red Bank Road, Wisteria Road and Pandora Drive.



Community Gardens at Townhomes - This amenity is a linear park located within the townhome development. The park provides paths, seating, and perennial gardens for the residents and visitor to enjoy.



Dog park - Located on the southwestern portion of the single family development, this park provides a shared open space for the community.



A Property Owners Association will be created for the planned development which would manage and maintain the roadways, the drainage system, trails, paths and all other common/amenity areas including open space.

Images above are intended to be for illustrative purpose and are representational ONLY for proposed amenities

8. DESIGN GUIDELINES

Avalon Point Planned Development master plan has been developed in consideration to topography, existing environmental conditions, surrounding community and aesthetics. Unless otherwise stated, the criteria that follows below will be the guiding principles for the City of Goose Creek planning and zoning staff to review and approve building and site plans.

The following guidelines include architectural standards for residential and commercial buildings, parking, landscape, tree mitigation, signage and amenities.

Architectural Standards

8.a. Single Family/Townhome Overview: The following images below represent the intent and direction of the building materials for the single family detached and attached development.

- Horizontal Vinyl Siding and Trim Boards
- Shake and Board & Batten Details
- Louver, Solid and None Shutter Options
- Stone and Brick Wainscot Options
- Stone Base Columns Options
- Dark Architectural Roof Shingles



Example of Single Family homes and Townhomes

8. DESIGN GUIDELINES

8.b Commercial Overview:

The following standards are established to guide appropriate building design for commercial properties within the Avalon Point Planned Development and, more specifically, to support the City of Goose Creek's Red Bank District. Material selections and design requirements have been implemented to ensure quality and durability. Land Uses for the commercial development are listed below.

- Arts/Crafts or dance studios/art galleries
- Microbrewery
- Retail, commercial, grocery, home furnishings, hardware (having less than 5,000 SF of floor area)
- Pet shop (retail sales only, no selling/boarding of animals)
- Radio/television studios
- Specialty shops, such as florist, gift shop, boutiques, coffee shop, bakery and the like
- Restaurants
- Vet Clinic/animal hospital without boarding
- Drive-thru Establishments permitted with City Zoning Administrator approval

The following uses shall be prohibited:

- Tobacco and Vape Shops
- Fast food restaurants with drive-thru facilities
- Non-depository personal credit institutions
- Vehicle sales, service, and/or repair including oil change shops
- Automatic and manual car washes
- Sexually Oriented Businesses
- Laundromats
- Auto Parts Stores
- Bail bonds
- Gas Stations

All other uses not identified within the land use listing above shall require City Zoning Administrator approval.



8. DESIGN GUIDELINES

Activating outdoor space to support retail, food & beverage, and other commercial businesses like yoga studios is an important requirement of these standards. Certain physical characteristics of the parcels—such as frontage along Red Bank Road for Parcel B or the mowed green, under the power easement at the back of Parcel A—lend themselves to pedestrian focused outdoor gathering areas. **Exhibit F** illustrates one way that activated outdoor spaces may be designed into the site and building plans.



Urban planners use the term “third spaces” to describe community gathering spaces that are not classified specifically as “work” or “residence”. A “third space” can take many forms. Some common examples are a **coffee shop, a casual food and beverage establishment or a unique retail shop, like a bookstore**. These base use businesses become meeting areas for organized events and may combine multiple uses together, such as a bookstore with a coffee shop. Organized events such as a poetry reading, or musical performance may be scheduled to bring patrons together. Another example may be a small plant-focused landscape nursery (as opposed to bulk materials) that includes a café and beer garden such as Kentucky Native Café in Lexington, KY. For the Avalon Point example, the left end cap for Building A could be a garden shop with interior plants and an outdoor plant nursery in the power easement. The shop could double as a café and host how-to events with refreshments in the outdoor patio.



Kentucky Native, Lexington KY

The goal of the developer is to attract at least one “third space” establishment within Avalon Point. To reinforce this desire, Avalon Point will create marketing materials to specifically highlight the opportunity to businesses that are catalysts for “third place” development. Prior to final approval of Avalon Point Boulevard, developer shall provide to the Zoning Administrator marketing materials that highlight the “third space” opportunity for Avalon Point.



Example of high-quality composite wood used as accent material for entry cover.

8. DESIGN GUIDELINES

Commercial Design Guidelines:

These design guidelines provide a certain degree of flexibility to allow for creative building design and inclusion of artistic elements such as sculpture and murals.



Example of corrugated metal, wood, awnings and two-tone paint schemes to create simple, attractive facades

Applications for commercial development shall provide a description on how the commercial design and site plan will meet the building massing, design, materials and site features. The City of Goose Creek staff will be responsible for reviewing and approving plans based on the design standards that follow:

OVERALL MASSING AND DESIGN

Buildings should be organized in a manner to engage the streetscape on Red Bank Road and Avalon Point Boulevard. The primary facades and pedestrian connections of the buildings should be along these two roads with the corner closest to the main intersection being the most architecturally significant. Garage access shall not be allowed to front Red Bank Road.

It is desirable to orient parking to the side or behind the building to promote a pleasant pedestrian experience. However, given regulatory limitations dictating the existing ditch easement crossing on Parcel A, parking may need to be placed in the front of the building. In this situation, design plans should take measures to limit the pedestrian impact through the use of



Example of blending murals and signage in an artistic blend and COR-TEN steel entry vestibule

8. DESIGN GUIDELINES

accentuated pedestrian crossings to Red Bank Road. Pedestrian connections to Red Bank Road are important to reinforce the City's plans to revitalize the Red Bank Road corridor.

Overall building design should be complimentary with the smaller commercial and residential scale along Red Bank Road. One-story buildings are permitted and the maximum building height is two stories or 35' to peak of building roof.

Building Materials

•Siding: A wide variety of traditional building siding materials are acceptable along with a list of contingent materials that are newer and more modern.

◇ Acceptable Materials

- Hardiplank
- Wood
- Brick, painted
- Board and batten
- Painted block
- Or other similar materials that meet the intent of these design guidelines w/ approval from the City Zoning Administrator.

◇ Contingent Materials: Other high-quality materials, such as modern panel systems and composite wood, may be approved as long as samples and specification sheets are provided that the materials are of durable quality and used in moderation. These materials are ideal for architectural accents.

•Visible Roofing: The material standards below are only applicable for pitched roofs with visible roof lines. Flat roofs shall include parapet walls to shield views of membrane and bituminous roofing.

◇ Acceptable Materials

- Architectural asphalt Shingles
- Non-reflective Metal Roofing, including double v crimp and standing seam

Building Design Characteristics

- Windows and Fenestration: Buildings shall have a ratio of 40-50% window-to-wall ratio for the facades facing Avalon Point Boulevard and Red Bank Road. Windows should be appropriately spaced and organized along the main facades. Windows are not to be flush mounted with the exterior walls. Windows are to be located as far inward into the wall to maximize depth and shadow. Storefront entries are encouraged to indicate the commercial nature of the building



Example of unique outdoor lighting

8. DESIGN GUIDELINES

and increase transparency and visibility.

- **Foundation:** Foundations should be at grade or near at grade to enable ADA accessibility without the need for extensive ramping. Site and building should be designed to eliminate the need for switchback ramps.
- **Overhangs:** Pitched roofing structures shall be designed to ensure overhangs of at least 12" to enhance architectural interest
- **Porches and Awnings:** Awnings or porches are required over building entrances. No fabric awnings allowed.
- **Painting:** Utilizing two-toned paint
- **Murals:** Murals are encouraged, especially on larger, visible, flat surfaces. Murals visible from Red Bank Road are of particular interest to reinforce the arts district.

Site Design Features:

- **Alternative parking:** To complement the spirit of the neighborhood and arts district, provide bicycle and golf cart parking to encourage easy trips from Avalon Point and surrounding neighborhoods. One (1) bike space per 1,000 Square Feet shall be provided.
- **Lighting:** Outdoor festival lighting is encouraged to create attractive, lively, night time environments. Festoon lights, string lighting and unique outdoor hanging fixtures are examples of acceptable outdoor lighting.
- **Community Seating areas:** Community seating areas may be specific for patrons of a certain business, or they may be shared amongst multiple businesses. **Exhibit F** shows one scenario with outdoor seating along Red Bank Road for Building B and seating on the back side of Building A. Outdoor seating areas may be hardscaped or crushed stone patios and decks with tables and chairs and could be covered or uncovered. Additional elements like water features or fire pits are permitted.

8. DESIGN GUIDELINES

8.c. Signage

Signage is necessary to enhance commercial viability and the Avalon Point PD encourages diversity of signage within the residential and commercial develop areas.

While more traditional residential and business signs are permitted, the Avalon Point PD shall permit signage solutions that incorporate three dimensional elements, murals or other artistic touches that signify the uniqueness of the Red Bank District. This will allow for a more organic evolution of signage.

Overall signage guidelines are as follows:

1. No sign shall interfere with the motorists' vision, nor simulate traffic control or emergency vehicle lighting.
2. Hazard and directional Signs less than four square feet in area, consisting of arrows, name and colors of the business, graphics or such words as "Step", "Fire", "Escape", "Danger" or similar symbols, shall not be included in computing maximum allowable sign area.
3. Individual businesses shall not have individual freestanding signs. Only one (1) freestanding sign will be permitted along Red Bank Road. A secondary residential freestanding sign shall be permitted at Wisteria Road Entrance.
4. Only one (1) freestanding residential sign will be permitted along Red Bank Road and one (1) freestanding residential sign will be permitted along Wisteria Road.
5. Illumination
 - a. No sign lighting is allowed which switches on and off intermittently, changes intensity and/or color or otherwise creates an illusion of flashing or movement;
 - b. All bare light bulbs, except bulbs less than 15 watts, shall be directed toward the face of the sign;
 - c. Signs within 50 feet of a residential district shall be shielded from casting glare into the district; and



Example of string lighting in a community seating area



Example of Neighborhood entry signage



Unique Hanging Sign



Example of a third place and varied signage

8. DESIGN GUIDELINES

- d. Signs within residential districts shall be shielded from casting illumination into residences within 100 feet
 - e. Signs may only be illuminated in one of the following ways: halo lit, reverse halo lit, using up/down lighting, and marquee lighting.
6. Height, setback and location.
- a. A projecting wall sign shall project no more than five feet from a building, have bottom ten feet (minimum) above grade and top 25 feet above grade or height of the building, whichever is lower and have no exposed guy wires;
 - b. Free-standing signs, awning signs, canopy signs, marquee signs and temporary signs shall have a minimum setback of ten feet from the street curb, and shall not be installed within, nor project into the vertical plane of, the street right-of-way;
 - c. Fascia signs may project no more than 18 inches from a wall, and extend no more than 18 inches above the parapet, eaves or building facade;
 - d. Roof mounted signs may project no more than ten feet above the highest point of the roof or parapet;
 - e. No sign shall be attached to or obstruct any fire escape or opening intended as a fire fighting point of ingress or egress, interfere with any opening required for legal ventilation or prevent free passage from one part of a roof to another;
 - f. Street furniture, such as benches, waste receptacles, fountains and the like shall not be used for advertising purposes; and
 - g. When a sign extends over sidewalks, walkways or other spaces accessible to pedestrians, the bottom of the sign structure shall be at least eight feet above the ground.
7. Construction of signs. All permanent signs shall conform to International Building Code and National Electrical Code requirements as to design, materials, support, installation, fixtures and wiring. Specifically, all permanent signs must be able to withstand a pressure of 24 pounds per square foot, which is equivalent to 110 mph wind. The Zoning Administrator may require certification by an engineer when he or she is uncertain that the proposed method of construction is adequate.
8. A sign permit is required through the City of Goose Creek prior to placing a sign within the City Limits. All signage plans must be submitted to the City of Goose Creek zoning and planning staff for review and approval in accordance with this section 8.c. prior to sign permit being issued.
9. The maximum area permissible for signs shall correlate to the base zoning district and applicable Red Bank Overlay District. To encourage creativity, there shall be no area limitations on application of painted wall signage and/or mural themed applications. Zoning Administrator and Staff shall review all requests for permits.

8. DESIGN GUIDELINES

8.d Parking Requirements

Single Family Residential Parking:

- Two off-street spaces shall be provided per residential lot. The minimum driveway length for each residential unit shall be 20 ft measured from back of sidewalk to front of garage door, as to not impede pedestrian travel on proposed site sidewalks. Additional on-street parking will be provided as generally identified per the master plan.

Townhome Residential Parking:

- Alley fed access to the garages at the Townhome development will be provided. Each Townhome unit will require two off-street spaces. The minimum driveway length for each unit shall be 20 ft. measured from edge of alleyway to face of garage door.
- Guest/overflow parking will be provided as on-street parking to benefit homeowners.



Commercial Parking Requirement

1. 1 space per 350 SF. Square footage excludes storage, hallways and stairwells/elevators.
2. Shared parking for commercial uses is permitted. Required parking for commercial uses shall be located within the 3.45 acres of commercial zone of Avalon Point.
3. *Parking Area and paving required for parking spaces.*
 - a. Each automobile parking space shall measure nine feet by 18 feet minimum (162 square feet), excluding any aisle or maneuvering space. However, the paved length of a 90-degree parking stall may be reduced to 17 feet, provided that the curbing or anchored concrete wheel stops are furnished at the edge of paving to allow the vehicle to overhang a landscaped area of at least five feet in width. Parking stall length shall not be reduced where the vehicle would overhang a sidewalk. Areas in public rights-of-way shall not be used in providing parking or maneuvering space.
 - b. Parking plans shall be submitted in the form of a scale drawing of the proposed parking and loading spaces, with landscaped areas and demonstrated conformance with these regulations.
 - c. Approved parking spaces shall be paved or pervious paving as design dictates. City of Goose Creek staff shall review and approve material selections for parking stalls and driveway aisles in accordance with section 8.d.



Images above are intended to be for illustrative purpose and are representational ONLY for proposed amenities

8. DESIGN GUIDELINES

4. *Parking requirements for the disabled person(s).*
 - a. All governmental buildings, public buildings, commercial facilities and places for public uses, including churches and private clubs, with the exception of single-family and two-family residences, shall have parking spaces designated and signs posted in accordance with the following requirements.
 - The international symbol of access to the physically disabled person(s) shall be permanently displayed (marked) on the ground for parking spaces that are reserved for such use. In addition, a sign bearing the international symbol of access to the physically disabled person(s) shall be posted facing each reserved parking space for the disabled person(s). Examples of the signs included one foot by one and one half foot in dimension, with the international symbol and “reserved parking” clearly marked on the sign.
 - Standard ADA parking spaces and van accessible spaces shall be provided in accordance to current federal guidelines. (2010 ADA standards for Accessible Design)

8.e. Building Height

Section 3 of this document describes building height. Building height is measured from the average foundation grade to the top of the roof. Unoccupied portions of building, including decorative cupolas and/or fireplace chimneys may exceed this height, with prior approval from City of Goose Creek staff.

8.f. 30' Red Bank Road Activity Zone

To meet the desire intent of the Red Bank District, the Avalon Point development will enhance the existing frontage with landscape and hardscape. Upon submittal of commercial site plans, the City of Goose Creek staff will review and approve the activity zone which shall include enhanced sidewalk, lighting, low ornamental walls, landscaping, walkway and trail connections and may include public art, outdoor terraces, and other signage elements in accordance to Section 8.c.

8.g. Landscaping & Buffering Requirements

Landscaping and buffering shall generally be in accordance with the Avalon Point master plan. The following are guidelines for landscape and buffer submission to be reviewed and approved by the City of Goose Creek staff.

1. Landscape design shall be integrated with the overall project design concept. Proposed landscaping shall be evaluated in relation to the existing natural landscape and to existing and proposed landscaping, including the landscape elements existing on adjacent properties and street rights-of-way, and in relation to adjacent buildings, existing or proposed.
2. The existing natural landscape character shall be preserved whenever possible. Greatest diligence and care shall be exercised to retain and protect existing trees in the proposed landscape design.

8. DESIGN GUIDELINES

3. All landscape areas shall provide an attractive transition to adjoining properties. The landscaping plans for the proposed development shall provide visually harmonious and compatible settings for structures on the development, with adjacent properties, and shall blend with the surrounding natural landscape.

4. Landscaping shall be generally planted between buildings and sidewalks and/or buildings and curbing, and between parking lots and driveways.

5. Location of trees shall be coordinated with the location of exterior lighting, security cameras and overhead power lines in order for trees to have room to mature and not conflict.

BUFFER:

An existing buffer shall exist to serve as a perimeter buffer between differing land uses adjacent to the planned development and between commercial and residential uses within the planned development. Landscaping plans shall be submitted to the City of Goose Creek staff for review and approval.

- Avalon Point PD- Residential adjacent to Residential (generally not required between like uses)
 - Proposed 10' vegetated buffer.
 - The 10' wide buffer shall consist of at least four canopy trees, eight understory trees, and 30 shrubs per 100 linear feet. This plant material can be made up of new or existing plant material.
- Avalon Point PD - Residential adjacent to Commercial
 - 20' vegetated buffer.
 - The 20' wide buffer shall consist of at least four canopy trees, eight understory trees, and 30 shrubs per 100 linear feet. This plant material can be made up of new or existing plant material.
 - Avalon Point PD - Residential adjacent to the Kennedy Center
 - 35' vegetated buffer.
 - Where buffer is impacted by land disturbance, planting shall be provided. The 35' wide buffer shall consist of at least four canopy trees, eight understory trees, and 30 shrubs per 100 linear feet. This plant material can be made up of new or existing plant material.
- Avalon Point PD – Commercial parcel adjacent to Residential property
 - 20' vegetated buffer.
 - The 20' wide buffer shall consist of at least four canopy trees, eight understory trees, and 30 shrubs per 100 linear feet. This plant material can be made up of new or existing plant material.
- Avalon Point PD – Commercial adjacent to Mt. Carmel Church Property:
 - An existing 13.5' Sanitary Sewer Easement exists at this abutment. Easement will remain and no additional buffer is required.
- Red Bank Road Activity Zone – 30'
- No required buffers between commercial land uses internal to Avalon Point PD
- No required buffers between single family attached or detached land uses internal to Avalon Point PD

8. DESIGN GUIDELINES

STREETSCAPE

Street trees shall be installed in a consistent rhythm and spacing generally illustrated in Exhibit E- Master Plan. All street trees and landscape tree caliper inches provided shall count towards mitigation inches. A minimum of 2.5" caliper canopy tree shall be required.

LANDSCAPE FOR PARKING LOTS

1. To create shade over the majority of a paved surface, the majority of the trees shall be canopy trees. Tree canopies soften the visual impact of parking areas and relieve them from heat build-up.
2. To provide a canopy, a planted island or break at least five feet wide with at least one tree and/or shrubs, groundcover, ornamental grasses, or two trees shall be installed for every ten spaces of parking area. Shall be no more than 10 continuous parking spaces without a tree island.
3. Parking islands shall extend the full length of the adjacent parking aisles.
4. Parking lots shall provide a minimum ten percent net area of landscaping on the interior or exterior of parking lots.
5. Tree islands shall be placed along the perimeter of the parking area at each end of the drive aisles and interspersed as needed.
6. Where vehicles overhang a foundation planting strip, the width of the planting strip must be expanded to six feet.
7. For parking lots exceeding 75,000 square feet, linear planting strips are required every 250 linear feet which shall include, in addition to the required trees, 15 shrubs per 100 linear feet. These are minimum standards; additional landscaping is encouraged.
8. All landscaped areas shall be protected from vehicular encroachment by concrete curb and gutter unless planted per best management practices for Low Impact Development site design.



Example of low impact development site design



Example of retention ponds and edges

Zoning Administrator shall review and approve landscape plans prior to approval of the final plat.

8. DESIGN GUIDELINES

LANDSCAPE FOR COMMERCIAL BUILDINGS

1. Landscape plans shall be generally in accordance with the design guidelines described in section 8 and shall include screening for equipment and refuse collection areas.

LANDSCAPE FOR RETENTION AREAS

1. Retention ponds shall be landscaped generally as illustrated in Exhibit E- Master Plan with plants and materials as recommended by best management practices for bio retention areas.
2. Plant materials shall be compatible with the use.
3. The slopes shall not exceed 3:1.

All single family residential front yards shall be landscaped prior to closing.

The Avalon Point Stormwater Ponds shall include a 5-foot perimeter path that may be constructed of a semi-pervious material.

8.i. Tree Removal and Protection

The proposed planned development is located on undeveloped parcels and the developer shall work toward preserving existing vegetation where possible. The following will be required:

Tree survey

A tree survey is required prior to any new development or substantial redevelopment, or when the Planning Director deems it necessary. The following information shall be shown on a tree survey:

1. Name, phone number, address, signature, and seal of a licensed surveyor, landscape architect, or civil engineer registered in the State of South Carolina;
2. Tree survey clearly identifying the location and species of protected and grand trees and whether those trees are to be preserved, relocated, or removed;
3. The boundaries of the proposed development, including the different phases of development;
4. The boundaries of the buildable areas, and any proposed structures and parking areas;
5. The location, species and DBH of trees eight inches or greater DBH;
6. For sites greater than ten acres, the survey may consist of all grand trees for the entire site and a detailed sample survey of at least one acre that is representative of the majority of the site. For the purposes of determining mitigation, the detailed sample will be considered in determining the number of protected trees per acre;
7. Tree protection methods and specifications;
8. Trees shall be true to name and variety established by the American Joint Committee on Horticultural Nomenclature "Standardized Plant Names";
9. Any other information as deemed necessary by the applicant or by the Planning Director.

8. DESIGN GUIDELINES

Tree protection.

The city expects that the following tree protection methods shall be followed. The intent is not to have the city hire an arborist in order to enforce any section of this section. It is the responsibility of every applicant and his or her representative to conserve and protect trees according to the tree survey.

- a. Temporary protective barriers shall be erected at least up to the drip lines of trees to be saved.
- b. Provide protection fencing for trees designated to remain. The fencing shall be orange safety fencing at least three feet high supported by wood or metal poles.
- c. There shall be no construction, paving, operation of equipment or vehicles, or storage and dumping of materials within this protected zone. Where grading must occur, trees shall be protected.
- d. Temporary protective barriers shall be maintained until the issuance of the certificate of occupancy.
- e. No fill material shall be placed within the drip line of any tree in excess of acceptable level for the particular species.
- f. Landscaping activities taking place after the removal of temporary protective barriers shall be accomplished with light machinery or by hand.
- g. The Planning Director shall be notified prior to any deviation from the above-mentioned criteria.
- h. Protected trees shall be marked with surveyor's flagging for easy identification and inspection.
- i. Corrective measures shall be taken for trees in declining health. Such measures may include:
 1. Initiation of pest control measures;
 2. Pruning for good forestry;
 3. Fertilizing to restore vigor; and
 4. Other mitigation measures as deemed necessary.

Tree replacement, relocation and mitigation.

The intent of this section is to create conscientiousness in tree conservation and protection. The objective is to retain existing trees as much as possible, replant reasonably larger trees and provide alternate methods for tree replacement. Trees shall be mitigated according to the specifications provided herein.

- a. Each protected or grand tree that is determined by planning and zoning staff to be hazardous, diseased or injured to the extent it is irreparably damaged shall be approved for removal. The burden of providing proof of the extent of the hazard, disease or injury shall rest with the applicant, who must provide documentation from a certified arborist.
- b. Each protected or grand tree that is determined by planning and zoning staff to conflict with the implementation of this master plan shall be approved for removal.
- c. Removal of protected or grand trees shall be mitigated at 100%. Removal of the protected or grand trees in accordance with an approved site development plan shall be replaced by the applicant on the property through the replanting of species approved by the city at a minimum of four inch caliper to meet mitigation.

8. DESIGN GUIDELINES

1. All existing trees including significant trees to be saved within the buildable area may count toward the total number of replacement trees required; and
 2. Replacement trees may be the predominant species on the property, or of species approved by staff.
- c. A tree replacement schedule is required showing the location, species and sizes of any replacement trees to be planted.
- d. In areas outside the buildable areas (yard areas), with the exception of significant trees, all trees over four inches DBH shall be saved when practicable. This does not preclude an applicant from removing trees that unnecessarily hinder the development process, or trees that are in the way of driveways, easements and the like. The Planning Director shall be consulted prior to removing any trees.
- e. Prior to issuance of a Certificate of Occupancy for a completed structure, the Planning Director shall provide a compliance inspection including the provisions in this PD. It is the responsibility of the owner or agent to contact the Planning Director regarding the compliance inspection. The Certificate of Occupancy will be withheld pending verification of compliance. The Planning Director shall approve a delayed schedule for planting materials when the immediate planting schedule would impair the health of the plants. When a delayed planting schedule is approved, the applicant shall provide a bond equivalent to one and one-half times the projected cost of the planting materials.
1. All trees planted, replanted, relocated or mitigated may count toward the landscaping requirement for trees. The City of Goose Creek encourages the planting of trees as an integral part of the landscaping design of a development. Tree planting shall be at least 10% of the landscape design of a project. The use of native trees is preferred.

EXHIBIT A - LOCATION MAP



EXHIBIT B - EXISTING ZONING MAP



1 inch = 333 feet
 1 in
 Date: 11/8/2021
 Berkeley County GIS



The county of Berkeley and its GIS Department disclaims accountability for this product and makes no warranty express or implied concerning the accuracy thereof. Responsibility for interpretation and application of this product lies with the user.

EXHIBIT D - PROPOSED PLANNED DEVELOPMENT LAND USE



- LEGEND:**
- COMMERCIAL USE
 - COMMERCIAL USE - GREEN SPACE
 - RESIDENTIAL USE
 - RESIDENTIAL USE - GREEN SPACE
 - RESIDENTIAL ATTACHED USE

EXHIBIT E - MASTER PLAN



Legend

- Red Bank Road Access
- Red Bank Road 30' Activity Zone
- Commercial Frontage
- Wisteria Road Access
- Townhome Neighborhood
- Community Garden
- Single Family Neighborhood
- Single Family Amenities:
 - Loop Trail
 - Wooden Bridge
 - Fenced Dog Run
- Townhome Amenities:
 - Trail connections
 - Picnic Shelter
 - Fire Station
 - Playground
- 100' Community Greenway
- Loop Trail
- Pandora Road Trailhead

EXHIBIT F - COMMERCIAL



EXHIBIT G - CENTRAL COMMUNITY AMENITY



EXHIBIT H - GARDEN FRONTAGE TOWNHOMES



- 1 ON STREET PARKING
- 2 LOOP TRAIL CROSSING
- 3 COMMUNITY GARDENS
- 4 SEATING AREAS
- 5 GARDEN GATEWAY

EXHIBIT I - FOREST CLEARING DOG PARK



- 1 PEDESTRIAN BRIDGE
- 2 LOOP TRAIL
- 3 ENHANCED STREAM EDGE
- 4 PRESEVERED FOREST
- 5 SEATING AREA
- 6 ENCLOSED DOG RUN

THIS IS EXHIBIT IS CONCEPTUAL AND SUBJECT TO CHANGE

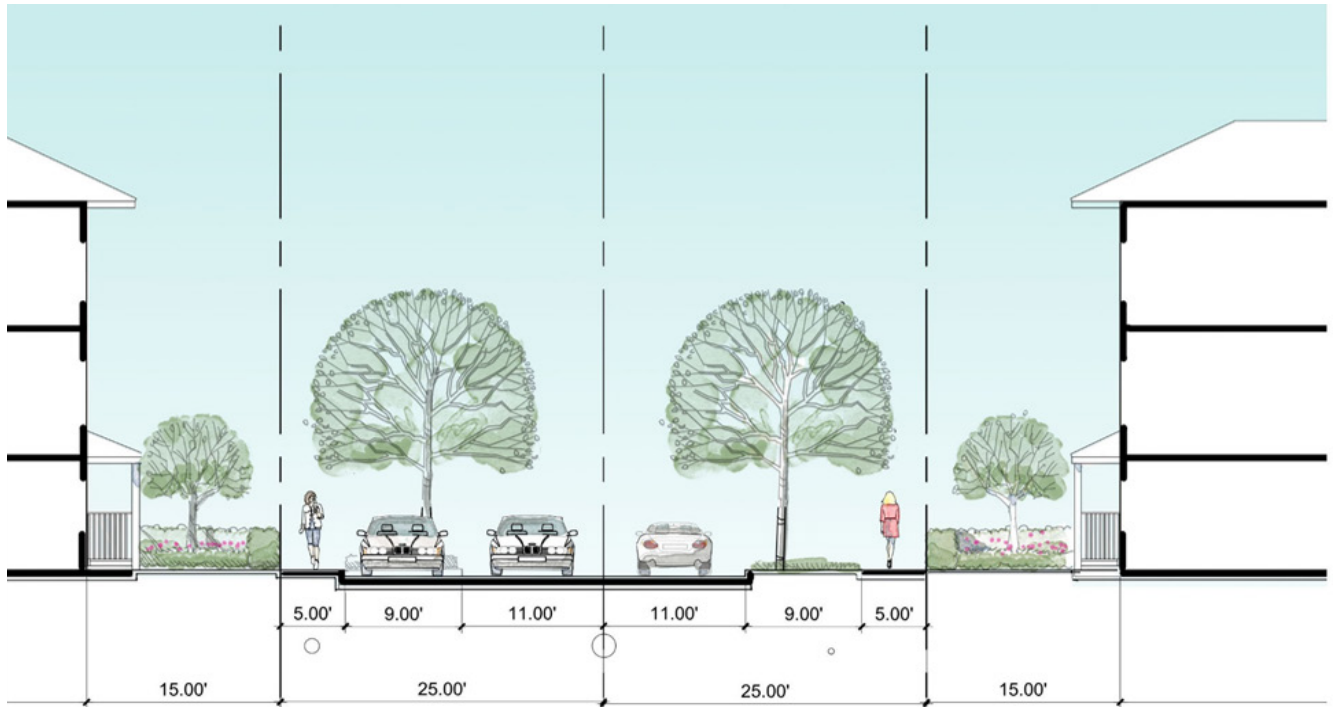
EXHIBIT J - BUFFER



Legend

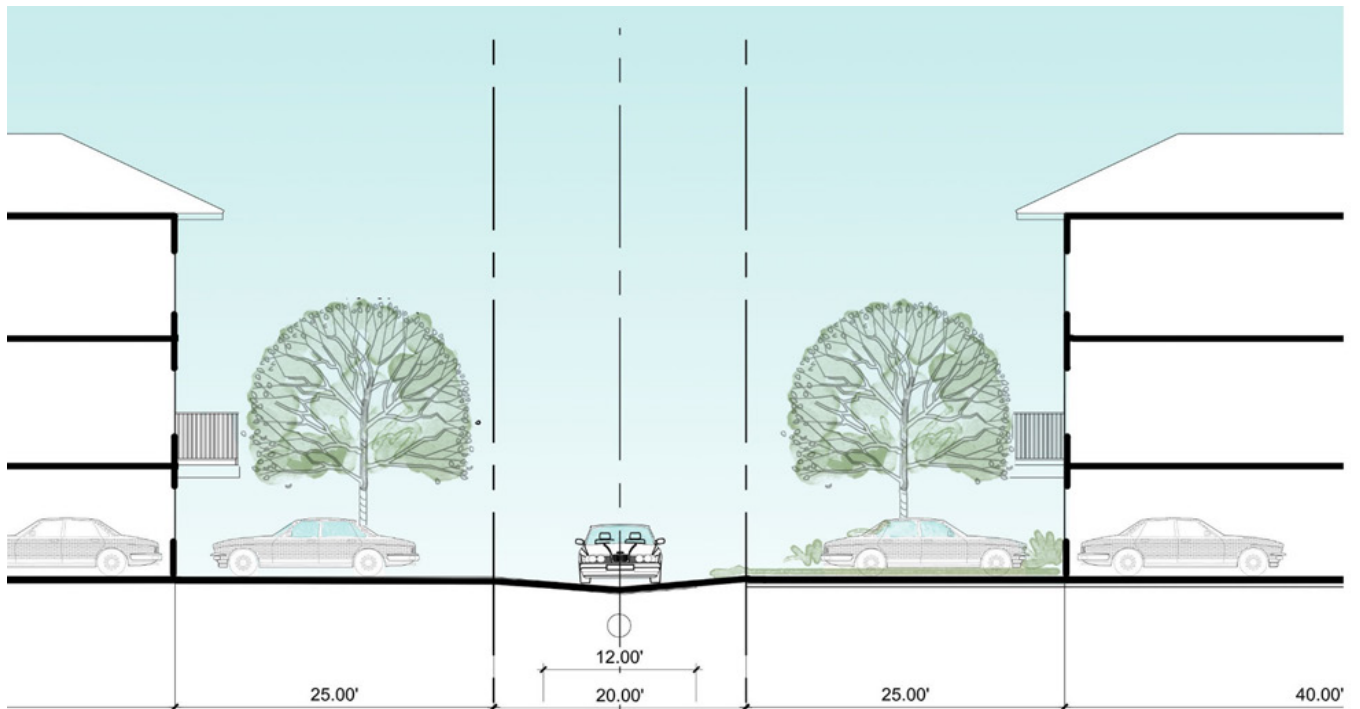
- 1 10' BUFFER
- 2 20' BUFFER
- 3 35' BUFFER
- 4 15' WETLAND BUFFER
- 5 WETLAND
- 6 HOA AMENITY AREA
- 7 EXISTING EASEMENT
- 8 30' RED BANK ACTIVITY ZONE

EXHIBIT K - TYPICAL 50' RIGHT OF WAY SECTION



TYPICAL ROAD SECTION THIS IS EXHIBIT IS CONCEPTUAL AND SUBJECT TO CHANGE

EXHIBIT K - TYPICAL 20' ALLEY RIGHT OF WAY SECTION



TYPICAL ALLEY SECTION THIS IS EXHIBIT IS CONCEPTUAL AND SUBJECT TO CHANGE

EXHIBIT L - WATER AVAILABILITY LETTER



THE CITY OF
GOOSE CREEK
BERKELEY CO. EST. 1961 SO. CAROLINA

Chuck Denson
DIRECTOR
DEPARTMENT OF PUBLIC WORKS

200 BUTTON HALL AVENUE
P.O. DRAWER 1768
GOOSE CREEK, SC 29445-1768
TEL (843) 824-2200
FAX (843) 863-5218

February 2, 2022

Josh Lilly - P.E.
Stantec
4969 Centre Pointe Drive
Suite 200
North Charleston, SC 29418-6952

Re: TMS# 244-09-02-008

Dear Mr. Lilly:

The City of Goose Creek has the water system capacity and the ability to serve the subject project. The owner will be responsible for all engineering design, utility construction and regulatory permitting associated with extending the water distribution system infrastructure to this project, including the transfer of ownership to the City. The City of Goose Creek will assume the operation and maintenance of this system once construction is completed and SCDHEC approval is obtained.

Please contact me at 824-2200 ext. 4263 should you have questions or require additional information.

Sincerely,

A handwritten signature in blue ink that reads "C. E. Denson".

Chuck Denson, P.E.
Director of Public Works

cc: City Administrator
O&M Manager
Planning Department
Project File

EXHIBIT M - SEWER AVAILABILITY LETTER



212 Oakley Plantation Dr.
Moncks Corner, SC 29461
bcws.berkeleycountysc.gov

February 22, 2022

Bryan Kizer
117 Red Bank Rd.
Goose Creek, SC 29445

Re: Application for Sewer Service, TMS # 244-09-02-008; 117 Red Bank Rd.

Dear Customer:

We have on hand your residential application for sewer service. At this time, sewer is available to the above location. However, the owner would need to have approved plans and fees paid before any connections would be approved.

If you have any questions, please contact our Engineering Department at (843) 719-2317 or you may email bcws.eng@berkeleycountysc.gov.

Sincerely,



Taps Department
BCWS Billing: Taps
T: 843-719-2644
taps@berkeleycountysc.gov
www.berkeleycountysc.gov
212 Oakley Plantation Dr Moncks Corner, SC 29461

BERKELEY COUNTY SOUTH CAROLINA



EXHIBIT N - SANTEE COOPER CORRESPONDENCE

From: [Mahoney, Christopher](#)
To: [Lilly, Josh](#)
Cc: [Horne, Jenny](#); [Tom Wallington](#)
Subject: RE: Red Bank
Date: Thursday, May 26, 2022 3:35:23 PM
Attachments: [image002.png](#)
[image003.png](#)

Josh,

That is great news! Are we ready to start the formal review? If so please submit through our portal if you haven't already the link is in my signature "Utility Encroachment Management" and add the TEFIS numbers to the drawings. We will also need to see a sheet showing existing vs proposed grading, utility plan, landscape plan, lighting, etc.

Let me know if you have any questions.

Best Regards,

Chris Mahoney | Supervisor | Right Of Way Management – OC07 | ISA Certified Arborist:SO-10504A
Santee Cooper | 1 Riverwood Drive – P.O. Box 2946101, Moncks Corner, SC 29461
☎ 843.761.8000 ex.5918 | ✉ chris.mahoney@santeecooper.com
📁 [Utility Encroachment Management | Santee Cooper](#)



EXHIBIT O - BERKELEY COUNTY SCHOOL DISTRICT CORRESPONDENCE



AVON TRACT

2022
February

Project No:
215616841

DRAFT

PREPARED FOR: **BEAZER HOMES - CHARLESTON** 4401 BELLE OAKS, SUITE 120 // N. CHARLESTON, SC, 29405

TRAFFIC IMPACT ANALYSIS

ALONG RED BANK ROAD
IN BERKELEY, SOUTH CAROLINA

PREPARED BY: STANTEC CONSULTING SERVICES INC. // N. CHARLESTON, SC

